

Appendix E

Department of Parks and Recreation Forms

State of California – The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 6Z

Other Listings _____
Review Code _____ Reviewer _____ Date _____

*Resource Name or # (Assigned by recorder) 15730/ 15732/ 15735 Lake Arthur Road

P1. Other Identifier: Map Reference #1

*P2. Location: Not for Publication Unrestricted
and (P2b and P2c or P2d. Attach a Location Map as necessary.)

*a. County Placer

*b. USGS 7.5' Quad Auburn Date 1953 Photorevised 1981T13N; R 9E; ___ ¼ of Sec 19; _____ B.M.

c. Address 15730/ 15732/ 15735 Lake Arthur Road City Applegate Zip 95703

d. UTM: (give more than one for large and/or linear resources) Zone _____; _____ mE/ _____ mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

APN: 077-120-052

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

A residential compound is located at 15730 Lake Arthur Road, at the southwest intersection of Lake Arthur and Placer Hills Roads. The property consists of three single family residences, an outbuilding, and two sheds situated on a 1.6-acre parcel. Landscaping on the property consists of a lawn with large trees and shrubbery surrounding the buildings and throughout the property.

The complex is situated on a remnant parcel created by the Division of Highways (now Caltrans) when it built the widened and realigned U.S. 40 (now Interstate 80) between 1949 and 1951. The northern eastern edge of the parcel is Placer Hills Road, which also serves as the Clipper Gap overpass over I-80. The eastern edge of the parcel is the westbound on-ramp for I-80, which is heavily bermed; the residential complex is sheltered by that high berm. The western edge of the parcel is Lake Arthur Road. There was a Lake Arthur Road before the Interstate was built but it was realigned and widened when the Interstate was constructed. (See continuation sheet).

*P3b. Resource Attributes: (List attributes and codes) HP-3, Multiple Family Property

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5b. Description of Photo: (View, date, accession #) 15730 Lake Arthur Road, camera facing northeast, September 23, 2010

*P6. Date Constructed/Age and Sources:

Historic Prehistoric Both
ca. 1889, ca. 1915, ca. 1930

*P7. Owner and Address:

Ronald L. and Doris C. Bailey
461 Julie Way
Applegate, CA 95703

*P8. Recorded by: (Name, affiliation, address)

K. Haley/ S. Mikesell
ICF
630 K Street, Suite 400
Sacramento, CA 95814

*P9. Date Recorded: 9/23/10 and 4/8/11

*P10. Survey Type: (Describe) Intensive



*P11. Report Citation: (Cite survey report and other sources, or enter "none.") ICF. 2011. Cultural Resources Inventory Report. *Applegate Wastewater Treatment Plant Closure and Pipeline Project. Placer County, California.* April. (ICF 00201.08) Sacramento, CA. Prepared for Placer County Department of Facilities Services, Placer County, CA.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (list) _____

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*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) 15730/ 15732/ 15735 Lake Arthur Road

B1. Historic Name: N/A

B2. Common Name: N/A

B3. Original Use: Residence B4. Present Use: Residence

*B5. Architectural Style: Italianate

*B6. Construction History: (Construction date, alteration, and date of alterations) ca. 1889, ca. 1915, ca. 1930

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: N/A

B9. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme Community Development Area Applegate

Period of Significance ca. 1889 Property Type Residential Applicable Criteria C

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

This property does not appear to meet the criteria for listing in the National Register of Historic Places because it lacks integrity. It is an improbable collection of three residences from very different period of construction: ca. 1889; ca. 1915; and ca. 1930, joined by more recently constructed buildings. The oldest home is original to this site; the other buildings may have been moved here. The real loss of integrity, however, relates to the freeway berms that dominate two of four sides of the parcel, and detract significantly from the integrity of setting, feeling, and association for this property.

This property is located in an area known as Clipper Gap; a community that grew with construction of the transcontinental railroad in the mid 1860s. Clipper Gap is a place name today but it was a small community with a railroad station, post office, and other signs of community in the 19th and early 20th centuries. For various reasons, the property around Clipper Gap developed into an industrial zone, although some agricultural developed in the area as well. In 1880, for example, a San Francisco company opened an iron mine "about three and a half miles" from the Clipper Gap station.¹

(See continuation sheet).

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References: See footnotes

B13. Remarks:

*B14. Evaluator: S. Mikesell

*Date of Evaluation: April 11, 2011

(This space reserved for official comments.)



¹ San Francisco Chronicle, July 27, 1880. W. B. Lardner and M. J. Brock, *History of Placer and Nevada Counties* Los Angeles, 1924, p. 197.

***P3a. Description (cont'd):**

A single family, two story Italianate residence is located at the center of the property. The asymmetrical-plan residence is supported by a concrete perimeter foundation and features a medium pitched pyramidal hipped roof which forms short closed eaves with slight overhang. The roof is clad with diagonally laid shingles. The residence is sided in horizontal clapboards. A single story full length porch under a hipped roof extension covered with diamond pattern shingles is located on the south elevation, (main façade). The porch with squared wood column supports and half wall enclosures clad with narrow horizontal wood siding is topped with a wood balustrade. A single story porch with wood supports is located on the north side of the residence. The north-side porch features a single entry door fronted by a security door accessed by wood steps. Fenestration on the residence consists of original wood 2/2 sash units with plain lintel throughout. A narrow brick chimney is located at the roof slope. County assessor records give an "effective date" of 1922 but reliable documentation about the building indicates it was built in 1889.

A second residence is located northwest of the main residence. It is a single story building with a medium pitched cross gabled roof clad with composition material. Fenestration on the residence appears to be replacement vinyl and a louvered vent is located at the gable on the south elevation. This building is given an "effective date of 1923" by the County Assessor. It is a Craftsman-designed home, indicating a likely date of construction in the years just before World War I.

A third residence is located on the property, northwest of the main residence. The building is supported by a concrete perimeter foundation and features a medium pitched gabled roof which forms short eaves on the eave walls. The roof is clad with corrugated metal cladding. The residence walls are sheathed with clapboard siding. An entry is visible on the east elevation, accessed by full-width concrete steps. Visible fenestration appears to be original wood sash with plain lintel. An air conditioning unit is affixed to the window on the south elevation. County records offer a date of 1930 for this building, which may be accurate, given the Minimal Traditional design of the building.

A shed is located on the property, northwest of the main residence. The shed has a medium pitched gabled roof which forms short closed eaves and is covered with horizontally laid tile shingles. The shed walls are clad with clapboard siding. A louvered vent is located at the gable on the east elevation.

A small wood outbuilding and small wood shed are located on the property. The shed features materials consistent with those on the second residence. County records indicate one of these buildings was constructed in 1969.

***B10. Significance (cont'd):**

A 1915 map of land ownership in Placer County provides a snapshot of industrial uses in the vicinity of the Clipper Gap railroad station. Pacific Portland Cement Company owned several section of land south and east of the station. Pacific Gas and Electric Company owned several large parcels just north of Clipper Gap. The land now occupied by this property on Lake Arthur Road was owned by the Giant Powder Company. It appears the powder company owned about 120 acres, including nearly all of the northwest quarter of Section 19 (where this property is located) and most of the southwest quarter of Section 18.

The Giant Powder Company was headquartered, first in San Francisco, then in Berkeley, before moving to Richmond in 1892. It left San Francisco and Berkeley following explosions at its facility, leading it to relocate to an isolated spot at Giant Avenue on Point Pinole in Richmond, the site of which is California Historical Landmark 1002-1. Landmark 1002 is the spot in San Francisco in Glen Canyon Park, the first dynamite factory that blew up a year after it had been built.² Giant is generally recognized as the first manufacturer of dynamite in the United States.

George Lay, past president of the Placer County Historical Society, argues the Giant Powder Company operated at Clipper Gap between 1889 and 1917.³ He maintains that there were many explosions at the plant, resulting in death of many people working at the plant.

² California State Parks, California Historical Landmarks, 1996.

³ Placer-Sierra Railroad Heritage Society Newsletter, May 2008, p. 2.

The oldest house at the Lake Arthur Road parcel – the two story Italianate structure – is directly related to the operations of the Giant Powder Company operations in Clipper Gap. As noted, the company was headquartered in the Bay Area but was likely under some pressure to move some or all of its manufacturing plant to a more remote location, owing to the high potential for deadly explosions at a dynamite plant. In the late 1880s, the company decided to open an auxiliary plant in Placer County which could supplement but not replace work at the main plant in Richmond.

The company hired a 24-year old from Ohio, Walter H. Gaffett, to design, build, and operate the Placer County plant. He selected Clipper Gap and began constructing various components of the mill in 1889.⁴ This included a bunkhouse where the workers lived, a company office, and the manufacturing plant, or mill. A final piece of construction there was the Gaffett House, which is the two-story home on Lake Arthur Road.

A photograph from the early 20th century shows the house and the mill property, included in a history of the small communities in central Placer County. The photograph shows the powder mill a short distance west of this house, along Lake Arthur Road. In another photograph in a 2005 article in the *Auburn Journal*, however, the newspaper attempts to overlay modern roads on a 1906 photograph. It indicates the bulk of mill operations were to the north along Placer Hills Road, not along Lake Arthur Road. Both photographs could be accurate because the powder company owned several hundred acres and there were good reasons to separate the operations from one another.

The powder mill operated episodically between the late 1880s and about 1917. There were problems with explosions. One local historian maintains there was one big explosion in 1891, shortly after the plant opened, killing three men. Another accident a year later killed two. A third blast occurred in 1895, killing one.⁵ The plant was also idled by a series of lawsuits involving Giant, Atlas, and Hercules, all California producers of dynamite, and the DuPont Company, which tried to shut them down for patent infringements. Agnes Mace, Walter Gaffett's daughter, contends the plant was idle between 1900 and about 1910, then reopened between 1910 and 1917 before it was closed permanently.⁶

Local press accounts say the plant was dismantled almost immediately after the plant closed in 1917.⁷ The history of this parcel is less well-documented from the period between the 1917 closure of the mill and the late 1940s construction of what is now Interstate 80.

The construction of the interstate spared the house but profoundly affected its setting. Before 1949, Lake Arthur was a through road, passing directly in front of this house and continuing east under what is now Interstate 80. A stub of the Old Lake Arthur alignment now serves as a drive way connecting this property with the realigned Lake Arthur. The other road in the area was Meadow Vista Road, now essentially displaced by Placer Hills Road. The new Placer Hills Road forms the north border for this parcel. The realigned and widened Lake Arthur Road forms the west side of the parcel and the westbound on ramp for Interstate 80 forms the east side of the parcel.

In summary, Interstate 80 and improvements to local roads to serve as overcrossings realigned all sides of the 1.6 acre parcel on which these houses sit. It was not a simple realignment because the realigned roads were made part of the raised overcrossing. The Clipper Gap overcrossing is Placer Hills Road and it is a huge berm, much taller than the tops of the two-story Gaffett House. The westbound on ramp, which forms the east boundary of the property is the same height as the overcrossing. The realigned Lake Arthur Road is at its original grade but is so wide as to be out of keeping with the property. Only the old Lake Arthur Road to the south is in keeping with the setting for this home, but it operates like a driveway, leading to the house before dead-ending into the westbound onramp.

⁴ Donna Howell, *Prose for Posterity: Placer Hills school distract communities: Applegate, Christian Valley, Clipper Gap, Meadow Vista, Weimar*, 2001. This is a privately published book sold by the Placer County Historical Society. Howell's information on Gaffett is documented in letters from Gaffett's daughter, Agnes Mace.

⁵ Bill G. Wilson, *Gold and Schemes and Unfulfilled Dreams*, Placer County Historical Museum 2003, pp. 353-354.

⁶ Howell, p. 50.

⁷ Tom Coghlan, "Explosive History," *Auburn Journal*, April 6, 2005.

The changes to the setting associated with the Interstate construction are well-documented. It appears that the setting has also been modified through the addition of several generations of other buildings on the parcel, in addition to the Gaffett home. Documentation has not been found to substantiate the point but it is likely that the 1915 and 1930 homes were added to this parcel well after the plant shut down in 1917.

On balance, taking into account the known changes to the setting from freeway construction and the visible changes to the setting from the 1915 and 1930 homes, it is concluded that the building and its parcel do not retain sufficient integrity to warrant listing in the National Register of Historic Places. The current owner, Doris Bailey, told a reporter for the Auburn Journal that the home was worth saving but she would never live in it because, "The character was spoilt when the y built the roads all around it." ⁸ Mrs. Bailey's comments neatly summarize why the property is not eligible for listing in the National Register.

Additionally, the property was evaluated in accordance with Section 15064.5(a) (2)-(3) of the CEQA Guidelines, using the criteria outlined in Section 5024.1 of the California Public Resources Code, and determined to not be a historical resource for the purposes of CEQA.

Photographs (cont'd):



Photograph 2: Residence 1, camera facing northeast

⁸ Tom Coghlan, "Explosive History," *Auburn Journal*, April 6, 2005.

Photographs (cont'd):



Photograph 3: Residence 2 (far left), Residence 3 (right) and shed (center), camera facing north



Photograph 4: Residence 2, Residence 3 and shed, camera facing north

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*Resource Name or # (Assigned by recorder) 15730/ 15732/ 15735 Lake Arthur Road

*Recorded by K. Haley/ S. Mikesell *Date September 23, 2010 Continuation Update

Photographs (cont'd):



Photograph 5: Main residence and berms, camera facing northeast

P1. Other Identifier: Map Reference # 2

***P2. Location:** Not for Publication Unrestricted

***a. County** Placer

and (P2b and P2c or P2d. Attach a Location Map as necessary.)

***b. USGS 7.5' Quad** Auburn **Date** 1953/PR 1981 **T** 13N; **R** 9E; ___ ¼ of Sec 17; _____ **B.M.**

c. Address N/A **City** Applegate **Zip** 95703

d. UTM: (give more than one for large and/or linear resources) **Zone** 10; 0672638 **mE/** 4316539 **mN**

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

***P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

An approximately 200 foot segment of the Boardman Canal (which extends through Placer County from the Cedar Creek Canal in Alta to Rocklin), is located in a rural portion of Applegate, in Placer County. Specifically, the subject segment extends north and south of Applegate Road, between Fairridge Drive on the east and Cheryl Lane on the west. The subject canal segment runs under Applegate Road. The Boardman Canal receives water from the Yuba and Bear River systems, located north of the subject segment. Water travels in a southwest direction.

The southeastern portion of the subject Boardman Canal segment (east of Applegate Road) is earthen in parts and lined with concrete in other parts. Much of the concrete has broken down or is cracked and is covered with overgrown riparian vegetation. This portion of the canal is approximately 3 feet deep and 6 feet wide at the top to 4 to 5 feet wide at the bottom. A contemporary concrete pump structure is located adjacent to the canal on the east side of the canal. It has a flat roof clad with composition material with pipes coming out of it and into the canal. A concrete bridge structure allows the canal to travel under the road. The northwestern portion of the subject canal segment (west of Applegate Road) is earthen lined and nearly covered by riparian vegetation. This portion of the canal segment is approximately 3 feet deep and is 3-4 feet wide.

***P3b. Resource Attributes:** (List attributes and codes) HP-20, Canal/ Aqueduct

***P4. Resources Present:** Building Structure Object Site District Element of District Other (Isolates, etc.)

P5b. Description of Photo: (View, date, accession #) Boardman Canal Segment, camera facing northwest, September 23, 2010

***P6. Date Constructed/Age and Sources:**
 Historic Prehistoric Both
ca. 1870 Placer County Water Agency; JRP Historical Consulting Services and Caltrans 2000

***P7. Owner and Address:**
Placer County Water Agency
P.O. Box 6570 Auburn, CA 95604

***P8. Recorded by:** (Name, affiliation, address)
Kathryn Haley
ICF International
630 K Street, Suite 400
Sacramento CA 95608

***P9. Date Recorded:** September 23, 2010

***P10. Survey Type:** (Describe) Intensive



***P11. Report Citation:** (Cite survey report and other sources, or enter "none.") ICF. 2011. Cultural Resources Inventory Report. *Applegate Wastewater Treatment Plant Closure and Pipeline Project. Placer County, California.* April. (ICF 00201.08) Sacramento, CA. Prepared for Placer County Department of Facilities Services, Placer County, CA.

***Attachments:** NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (list) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

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*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) Boardman Canal Segment

B1. Historic Name: Boardman Canal

B2. Common Name: Boardman Canal

B3. Original Use: Water Conveyance B4. Present Use: Water Conveyance

*B5. Architectural Style: N/A

*B6. Construction History: (Construction date, alteration, and date of alterations) ca.1890 continuous maintenance and upgrades.

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: N/A

B9. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme Water Conveyance Development Area Applegate

Period of Significance ca. 1870 Property Type Hydroelectric and Irrigation canal feeder Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The subject segment of the Boardman Canal does not appear to be eligible for listing in the NRHP due to a lack of historic integrity to its period of significance, ca. 1870 the year it was likely initially constructed.

The Boardman Canal is one of the largest canals in Placer County. Portions of this canal are listed in the NRHP. The canal was originally constructed in the early 1870s, for agricultural irrigation purposes (Angel 1882: 370). By the late 1800s the canal was owned and operated by the South Yuba Water Company and subsequently sold to PG&E in the early part of the 20th century. Both companies used the canal for hydroelectric power generation. In the 1980s PCWA took over ownership of a significant portion of PG&E's Placer County's water conveyance system in which the Boardman Canal is a part (Coleman 1952:229; Myer 2002:102; PCWA Update 1994; Placer County Water Agency 2005). (See Continuation Sheet).

(Sketch Map with north arrow required.)

See Location Map

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References: See Cultural Resources Inventory for full citations.

B13. Remarks:

*B14. Evaluator: Kathryn Haley

*Date of Evaluation: April 14, 2011

(This space reserved for official comments.)

L1. Historic and/or Common Name: Boardman Canal Segment

L2a. Portion Described: Entire Resource Segment Point Observation **Designation:**

b. Location of point or segment: (Provide UTM coordinates, legal description, and any other useful locational data. Show the area that has been field inspected on a Location Map.)

Zone 10; 0672638 mE/ 4316539 mN

Zone 10; 0672630 mE/ 4316559 mN

L3. Description: (Describe construction details, materials, and artifacts found at this segment/point. Provide plans/sections as appropriate.)

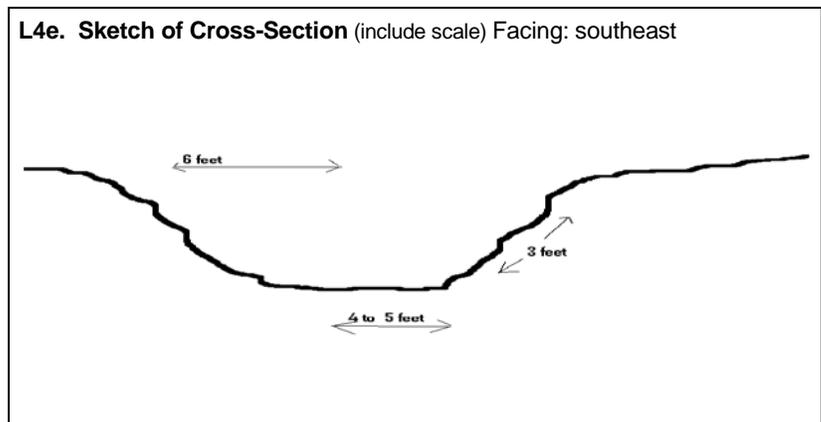
An approximately 200 foot segment of the Boardman Canal is located north and south of Applegate Road, between Fairidge Drive on the east and Cheryl Lane on the west. The subject canal segment runs under Applegate Road. The segment is earthen lined with portions covered with broken concrete, and rock. Overall the segment is surrounded by overgrown riparian vegetation.

L4. Dimensions: (In feet for historic features and meters for prehistoric features)

- a. Top Width:** Approx 6 ft
- b. Bottom Width:** Approx 4 to 5 ft
- c. Height or Depth:** Approx 3 ft
- d. Length of Segment:** 200 ft

L5. Associated Resources: N/A

L4e. Sketch of Cross-Section (include scale) Facing: southeast



L6. Setting: (Describe natural features, landscape characteristics, slope, etc., as appropriate.): The Boardman Canal segment travels in a north/south direction and extends under Applegate Road. The canal is located in a rural area of Placer County, just south of the town of Applegate. Riparian vegetation surrounds the canal segment.



L7. Integrity Considerations: Concrete gunite over the original earthen lining along with the addition of the contemporary pumping structure.

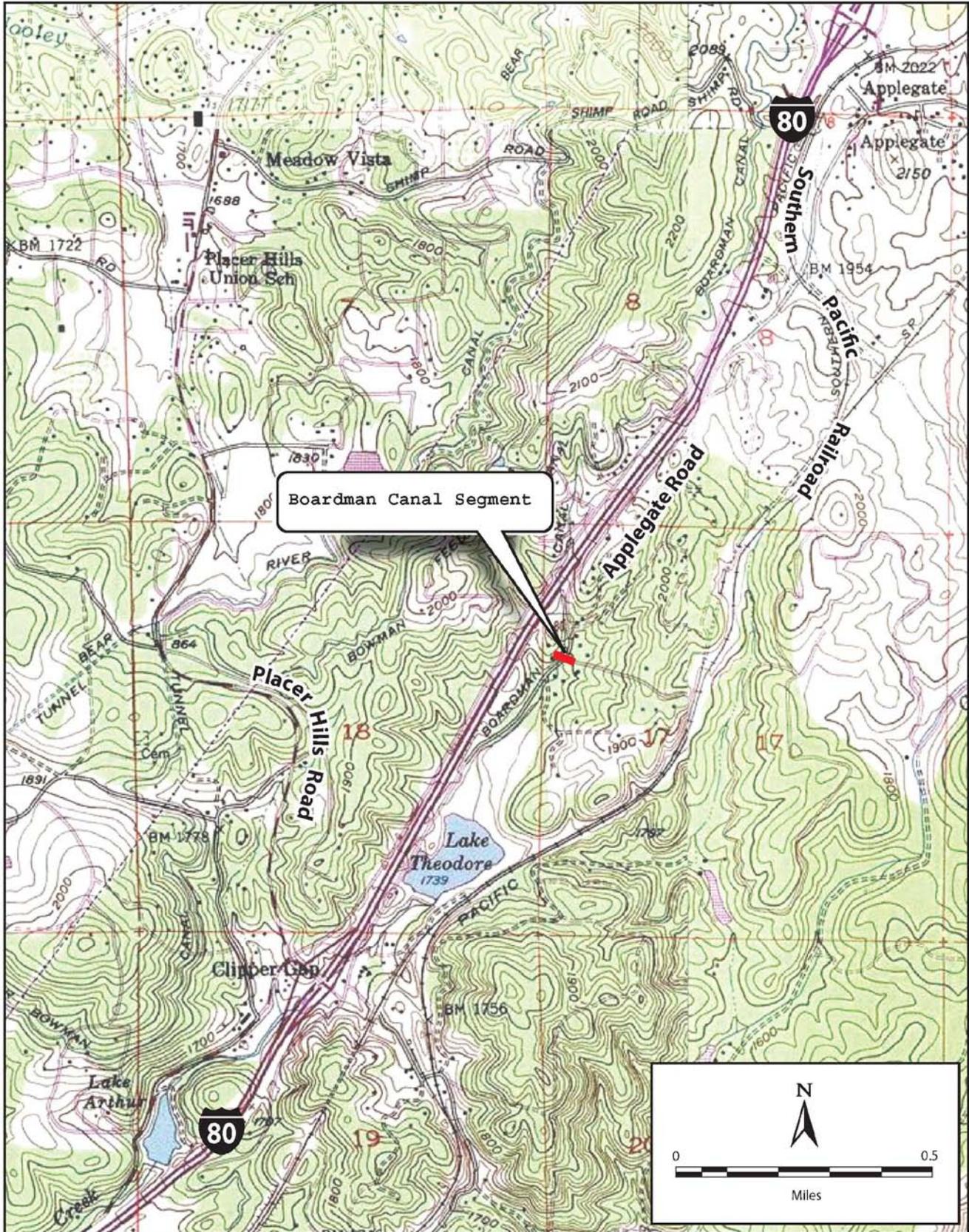
L8b. Description of Photo, Map, or Drawing (View, scale, etc.) Photograph 2, Pump, camera facing southeast

L9. Remarks:

L10. Form Prepared by: (Name, affiliation, and address)

Kathryn Haley
ICF International
600 K Street, Suite 300
Sacramento, CA, 95608

L11. Date: April 2011



***B10. Significance:**

The subject segment of the Boardman Canal appears to have been in continuous use since ca. 1870. It has been modified over the years to ensure its functionality to convey water for irrigation and hydroelectric power. Consequently, the segment of the canal has lost integrity to its period of significance due to alterations including the addition of gunite, and metal piping (under Applegate Road). Considering that the segment crosses under Applegate Road it is also highly likely that it was modified as a result of road construction related to the construction of and improvements to Applegate Road (formally part of the Lincoln Highway and Highway 40). From the standpoint of water system engineering, the canal segment is not distinct or exceptional; rather it was constructed in a manner common to canals throughout the state. The canal segment therefore does not appear to meet NRHP Criterion C. Overall, due to its loss of integrity, the segment of the Boardman Canal does not appear to meet criteria for listing in the NRHP.

Additionally, the property was evaluated in accordance with Section 15064.5(a) (2)-(3) of the CEQA Guidelines, using the criteria outlined in Section 5024.1 of the California Public Resources Code, and determined not to be a historical resource for the purposes of CEQA.

Photographs (cont'd):



Photograph 3: Applegate Road, Canal segment travels under the road. Camera facing northeast

Photographs (cont'd):



Photograph 4: Applegate Road and Boardman Canal Segment, camera facing south



Photograph 5: Boardman Canal Segment from Applegate Road, camera facing northwest

State of California – The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 6Z

Other Listings _____
Review Code _____ Reviewer _____ Date _____

*Resource Name or # (Assigned by recorder) Boardman Canal Feeder Segment

P1. Other Identifier: Map Reference # 3

*P2. Location: Not for Publication Unrestricted
and (P2b and P2c or P2d. Attach a Location Map as necessary.)

*a. County Placer

*b. USGS 7.5' Quad Auburn Date 1953 PR 1981 T13N; R 9E; 1/4 of Sec 8; _____ B.M.

c. Address N/A City Applegate Zip 95703

d. UTM: (give more than one for large and/or linear resources) Zone 10S; 0673222 mE/ 4317619 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The subject canal appears to be a feeder or overflow structure related to the Boardman canal. It is located in a rural area in Applegate, in Placer County. Specifically, the Feeder segment is located just east of Applegate Road and west of Bon Vue Road. Generally, the Feeder segment travels in a north/south direction. The subject canal segment is approximately 200 feet long and enclosed by a chain length fence. Directly adjacent to the canal is a contemporary CMU building and large water tank which appears to be a PCWA facility. This segment has a top width of approximately 6 to 5 feet, a bottom width of approximately 4 to 3 feet, and 3 feet deep. The segment is completely lined within gunite. A metal pipe measuring approximately 3 feet in diameter and a metal gate that spans the width of the segment are located at the northeast end of the segment. At that point it appears that the canal runs underground, under Applegate Road and under Interstate 80.

*P3b. Resource Attributes: (List attributes and codes) HP-20, Canal/ Aqueduct

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5b. Description of Photo: (View, date, accession #) Boardman Canal Feeder Segment, camera facing west, September 23, 2010

*P6. Date Constructed/Age and Sources:

Historic Prehistoric Both

Ca. 1890, Placer County Water Agency; JRP Historical Consulting Services and Caltrans 2000

*P7. Owner and Address:

Placer County Water Agency
P.O. Box 6570
Auburn, CA 95604

*P8. Recorded by: (Name, affiliation, address)

Kathryn Haley
ICF International
630 K Street, Suite 400
Sacramento CA 95608

*P9. Date Recorded: September 23, 2010

*P10. Survey Type: (Describe) Intensive



*P11. Report Citation: (Cite survey report and other sources, or enter "none.") ICF. 2011. Cultural Resources Inventory Report. *Applegate Wastewater Treatment Plant Closure and Pipeline Project. Placer County, California.* April. (ICF 00201.08) Sacramento, CA. Prepared for Placer County Department of Facilities Services, Placer County, CA.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (list) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

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*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) Boardman Canal Feeder Segment

B1. Historic Name: Boardman Canal

B2. Common Name: Boardman Canal

B3. Original Use: Water Conveyance B4. Present Use: Water Conveyance

*B5. Architectural Style: N/A

*B6. Construction History: (Construction date, alteration, and date of alterations) ca. 1890; lined with concrete gunite and metal pipe added, dates unknown.

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: N/A

B9. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme Water Conveyance Development Area Applegate

Period of Significance ca. 1890 Property Type canal Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

In 1996, Jones & Stokes Associates recorded and evaluated this canal segment as part of the *Archaeological Reconnaissance of the Proposed Applegate Water System Project* (see attached DPR). At that time it was given the Primary number P-31-2448-H. In this recordation the canal was documented as an overflow canal to the Boardman Canal which dated to the 1890s. Portions of Boardman Canal are listed in the NRHP. This segment was recommended as a contributing element of the Boardman Canal System. It appears that since 1996 the structure has undergone significant alterations to its setting including the addition of a contemporary building and water tank. It is inconceivable that the structure which is completely lined in concrete gunite and has a metal pipe which helps to extend the canal under Applegate Road retains any integrity of construction method, setting, feeling, or workmanship related to its period of significance ca. 1890. Consequently, the feeder/overflow structure related to the Boardman canal does not appear eligible for listing in the NRHP individually or as a contributing element to the Boardman Canal system.

Additionally, the property was evaluated in accordance with Section 15064.5(a) (2)-(3) of the CEQA Guidelines, using the criteria outlined in Section 5024.1 of the California Public Resources Code, and determined not to be a historical resource for the purposes of CEQA.

(Sketch Map with north arrow required.)

See Location Map

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References: See Cultural Resources Inventory for full citations.

B13. Remarks:

*B14. Evaluator: K. Haley, ICF International

*Date of Evaluation: April 14, 2011

(This space reserved for official comments.)

L1. **Historic and/or Common Name:** Boardman Canal Feeder

L2a. **Portion Described:** Entire Resource Segment Point Observation **Designation:**

b. **Location of point or segment:** (Provide UTM coordinates, legal description, and any other useful locational data. Show the area that has been field inspected on a Location Map.) Zone 10S; 0673222 mE/ 4317619 mN

L3. **Description:** (Describe construction details, materials, and artifacts found at this segment/point. Provide plans/sections as appropriate.)

An approximately 200 foot segment of the Boardman Feeder is located south of Applegate Road and west of Bon Vue Drive in Applegate. The segment is gunite lined and is surrounded by dry vegetation. General maintenance and repairs to canal lining have been conducted routinely overtime.

L4. **Dimensions:** (In feet for historic features and meters for prehistoric features)

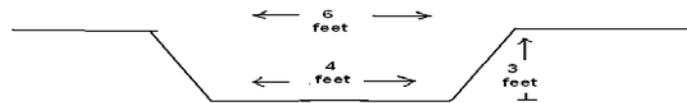
- a. **Top Width:** 6 ft
- b. **Bottom Width:** 4 ft
- c. **Height or Depth:** 3 ft
- d. **Length of Segment:** 18 ft

L5. **Associated Resources:** N/A

L6. **Setting:** (Describe natural features, landscape characteristics, slope, etc., as appropriate.):

The Boardman Feeder segment travels in a north/south direction. It is located directly northwest of Applegate Road. Directly adjacent to the canal (southeast) is a contemporary CMU building and large water tank which appears to be a PCWA facility.

L4e. **Sketch of Cross-Section** (include scale) Facing: southwest



L7. **Integrity Considerations:** Application of gunite over the original earth and concrete lining and the addition of a metal pipe and gate to the Feeder segment. Addition of modern building and water tank just southeast of the segment.

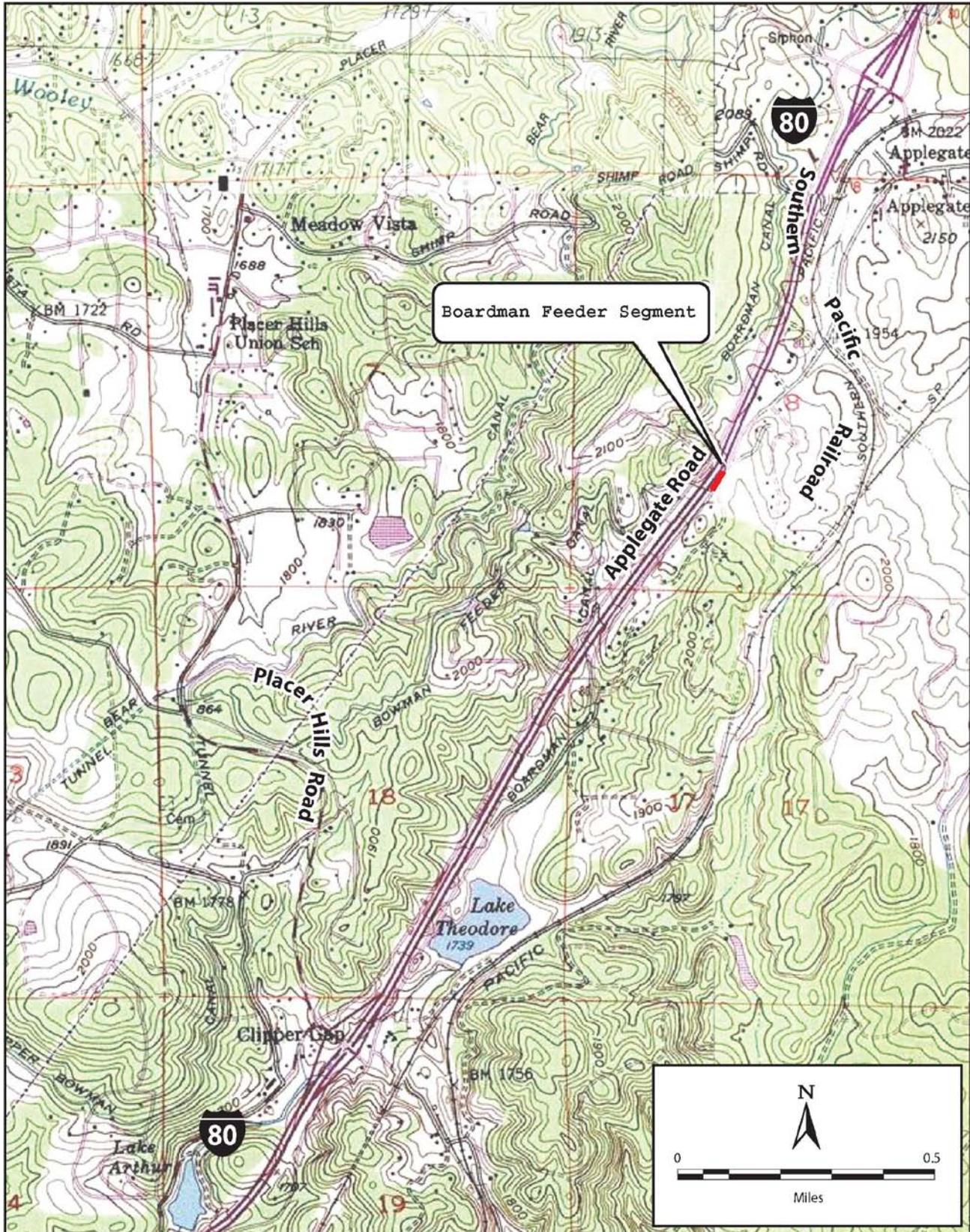
L8b. **Description of Photo, Map, or Drawing** (View, scale, etc.) Photograph 2, Boardman Canal Feeder segment, camera facing southwest

L9. **Remarks:**

L10. **Form Prepared by:** (Name, affiliation, and address)

Kathryn Haley
ICF International
600 K Street, Suite 300
Sacramento, CA, 95608

L11. **Date:** April 2011



PRIMARY RECORD

Primary # P-31-2448-H
 HRI # _____
 Trinomial _____
 NRHP Status Code _____

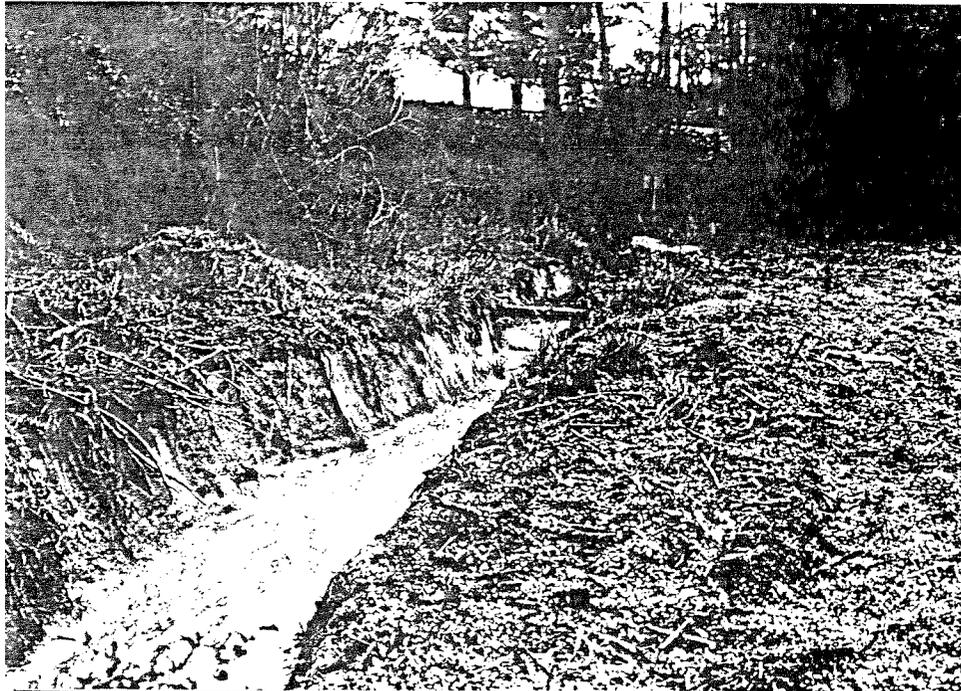
Page 1 of 4

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

P1. Resource Identifier: JSA-2
 P2. Location: a. County Placer and (Address and/or UTM Coordinates. Attach Location Map as required.)
 b. Address Bon Vue Road and Applegate Road
 City Applegate, CA Zip 95703
 c. UTM: USGS Quad Greenwood, CA (7.5) (7.5/15') Date 1973 ; Zone 10 , 673320 mE/ 4317370 mN
 d. Other Locational Data (e.g., parcel #, legal description, directions to resource, additional UTM's, etc., when appropriate):
673230 mE/ 4317730 mN to 673320 mE/ 4317370 mN

P3. Description (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.):
 The cement-lined canal runs along the western boundary of the project area. It is roughly 3 feet deep and 5 feet wide. There are several areas where erosion has undercut the cement, showing the lining to be roughly six inches thick. At the north end of the project area, the canal emerges from a corrugated metal pipe that apparently runs under Highway 80. On the other side of the freeway, near Pine Crest Drive, the overflow canal drops steeply from the Lower Boardman and makes a 90 degree turn to flow down a rather steep drainage. The canal is unlined before the water enters the corrugated metal pipe that apparently carries the it under the freeway. Metal gates, wooden footbridges and cement lining make up the intersection of these canals.

Resources present: Building Structure Object Site District Element of District



P6. Date Constructed/Age:
 Prehistoric Historic Both
1890s
 P7. Owner and Address:
Bert Espy
P.O. Box 68
Applegate, CA 95703
 P8. Recorded by (Name, affiliation, and address):
S. Ashkar
Jones & Stokes Assoc.
2600 V St., Ste 100
Sacramento, CA 95818
 P9. Date Recorded: March 5, 1996
 P10. Type of Survey: Intensive
 Reconnaissance Other
 Describe: Pedestrian Survey in 10 meter transects

P11. Report Citation (Provide full citation or enter "none"): Jones & Stokes Associates, Inc. 1996. Archaeological reconnaissance of the proposed Applegate Water System Project, Placer County, California. April 1996. Sacramento, CA. Prepared for MacIntosh Engineering and Development Company, Auburn, California.
 Comments: NONE Map Sheet Continuation Sheet Building, Structure, and Object Record Linear Resource Record
 Archaeological Record District Record Milling Station Record Rock Art Record Artifact Record Photograph Record
 Other (List): _____

5621

Page 2 of 4

- L1. Resource Identifier: JSA-2
L2. Historic Name: Overflow Canal of Boardman Canal
L3. Common Name: _____

L4. Detailed Record of: Entire Resource Segment (Describe entire resource on Primary Record before recording a segment in detail.)

L5. Length: approx. 2.5 miles Method of Determination: map

L6. Width: 5.25 feet (1.6 meters) Method of Determination: tape measure

L7. Depth/Height: approx. 3 feet Method of Determination: tape measure

L8. Features (Describe construction details, dimensions, and artifacts found with each feature. Provide plans/sections as appropriate.):

The cement-lined canal runs along the down a drainage of medium slope. It is roughly three feet deep and 5 feet wide. There are several areas where erosion has undercut the cement, showing the lining to be roughly 6 inches thick. The canal forms the western boundary of the project area and runs out of a corrugated metal pipe at the north end of the project area. The pipe carries the water under Highway 80. On the other side of the freeway, near Pine Crest Drive, the overflow canal drops steeply from a hairpin turn in the Lower Boardman Canal and makes a 90 degree turn to flow down a rather steep portion of the drainage. The canal is unlined before the water enters the corrugated pipe that carries it under the freeway.

L9. Natural Setting (Describe natural features, landscape characteristics, slope, etc. as appropriate.):

Mountain terrain of moderate to gentle slope. Drainage is thick with vegetation.

L10. Historical Information:

The portion of the Boardman Canal that this overflow canal stems from was built in the 1890s in response to the irrigation needs of horticulturalists (Lardner and Brock 1924:283-285). It was an extension of the Upper Boardman system which was built in 1865. The Boardman Canal was named after Arthur Flanders Boardman, a prominent Auburn resident and fruit grower.

L11. Resource Attributes (List attributes and codes.): HP-20

L12. Significance: Theme Water System Area Sierra Nevada Foothills
Period of Significance 1890s - present Property Type Canal Applicable Criteria A

(Discuss importance of resource within a historic context as defined by theme, period of significance, and geographic scope when appropriate.)

This resource is an overflow canal of the historic Boardman Canal, portions of which are listed on the National Register of Historic Places. The Boardman Canal system has played an important role in irrigation in the area from the early 1890s to the present day. The overflow canal appears to be a contributing element of the Boardman Canal System.

L13. Resource Integrity:

Though it has been improved (cement-lined), it retains its original function and alignment, giving it integrity of location and setting.

L14. Associated Resources Boardman Canal (CA-Pla-670H)

L15. References: Lardner, W.B. and M.J. Brock. 1924. History of Placer and Nevada Counties, California. Historic Record Company, Los Angeles.

L16. Form Prepared By: S. Ashkar Date 3/8/96

Affiliation and address: Jones & Stokes Assoc., Inc. 2600 V Street, Suite 100 Sacramento, CA 95818

Map Sheet

Primary # P-31-2448-M

HRI#/Trinomial _____

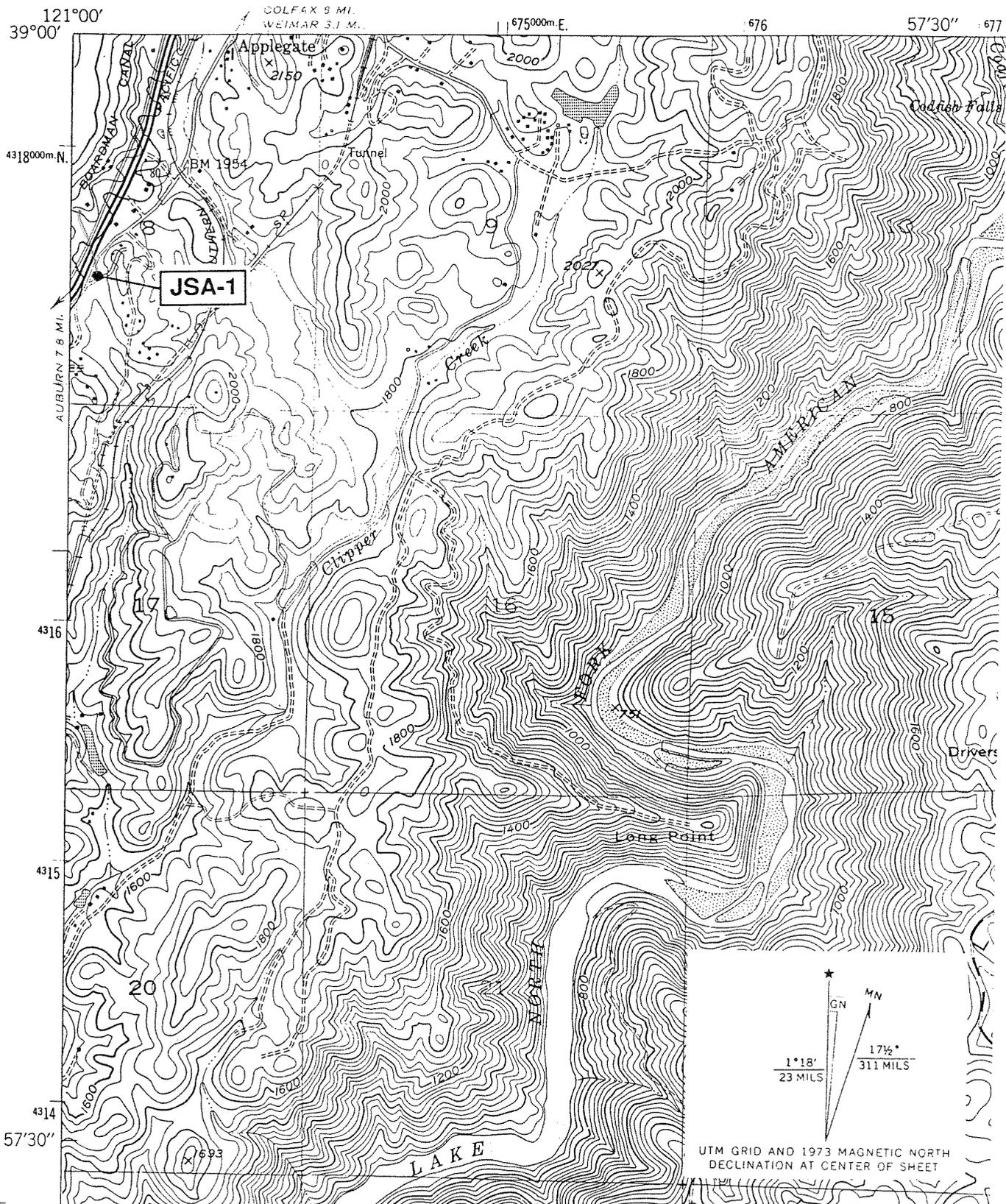
Page 3 of 4

Resource Identifier: JSA-1

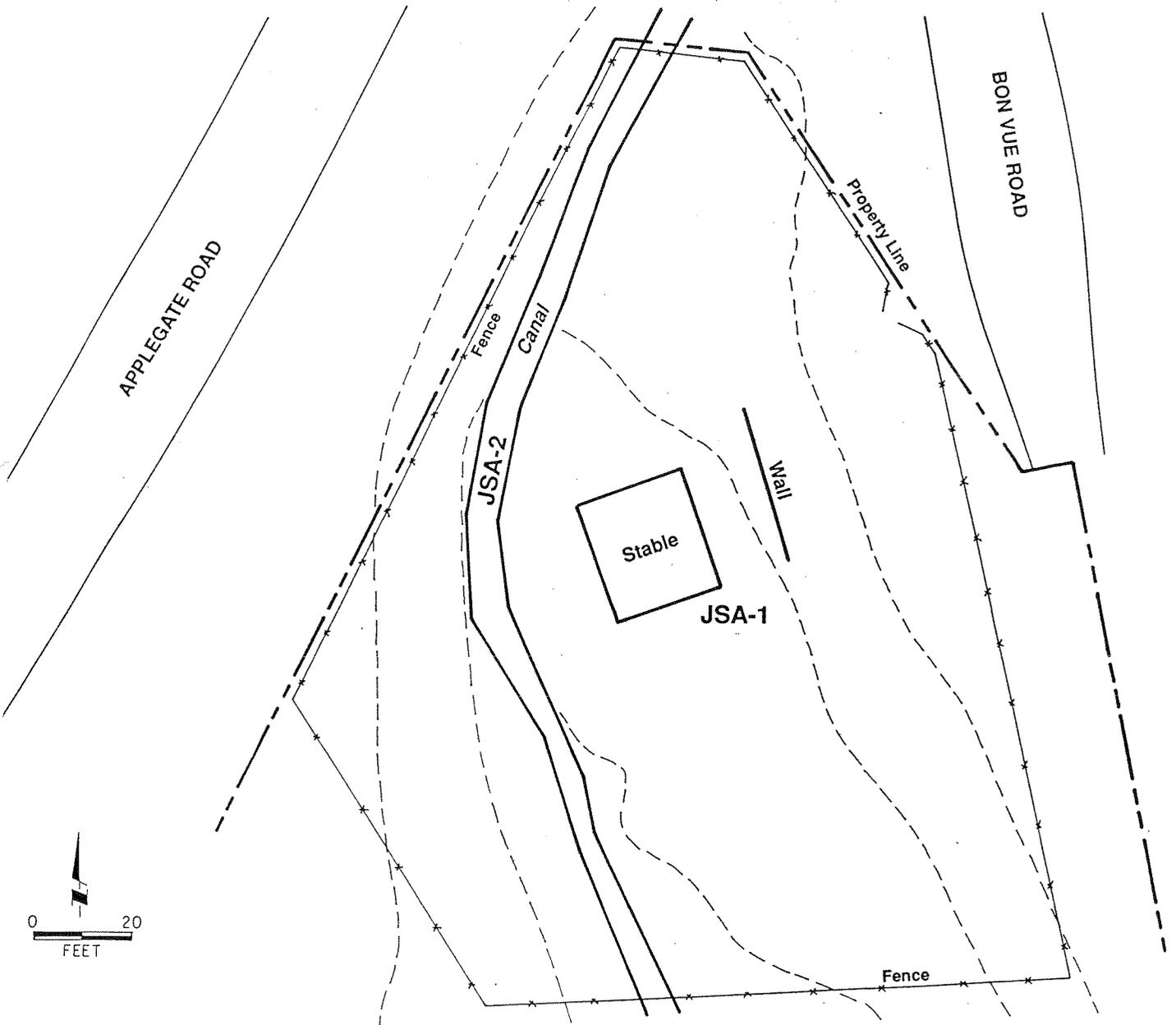
Map Name: Cultural Resource Location Map

Scale: _____

Date: _____



Map Sheet



State of California – The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 6Z

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or # (Assigned by recorder) Southern Pacific RR Bridge

P1. Other Identifier: Map Reference #4

*P2. Location: Not for Publication Unrestricted
and (P2b and P2c or P2d. Attach a Location Map as necessary.)

*a. County Placer

*b. USGS 7.5' Quad Greenwood Date 1949 PR1973 T13N; R 9E; $\frac{1}{4}$ of Sec 8; _____ B.M.

c. Address N/A City _____ Zip _____

d. UTM: (give more than one for large and/or linear resources) Zone 10S; 0673545 mE/ 4318088 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Southern Pacific (SP) railroad bridge is a deck plate girder bridge that carries the Union Pacific Railroad (UPRR) over Applegate Road in Placer County, (**Photograph 1**). The bridge is supported on two concrete seat abutments located at each approach which are on either side (northwest and southeast) of Applegate Road. The bridge is topped by a concrete deck with girders, wood beams and metal rails and ties.

*P3b. Resource Attributes: (List attributes and codes) HP19-Bridge

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5b. Description of Photo: (View, date, accession #) SPRR Bridge, camera facing north, September 23, 2010.

*P6. Date Constructed/Age and Sources:

Historic Prehistoric Both

1927, Southern Pacific Railroad Company

*P7. Owner and Address:

Union Pacific Railroad Company

*P8. Recorded by: (Name, affiliation, address)

Kathryn Haley
ICF International
630 K Street, Suite 400
Sacramento, CA 95814

*P9. Date Recorded: September 23, 2010

*P10. Survey Type: (Describe)

Intensive



*P11. Report Citation: (Cite survey report and other sources, or enter "none.") ICF. 2011. Cultural Resources Inventory Report. *Applegate Wastewater Treatment Plant Closure and Pipeline Project. Placer County, California.* April. (ICF 00201.08) Sacramento, CA. Prepared for Placer County Department of Facilities Services, Placer County, CA.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (list) _____

DPR 523A (1/95)

*Required Information

B1. Historic Name: SPRR Bridge

B2. Common Name: UPRR Bridge

B3. Original Use: Railroad Bridge B4. Present Use: Railroad Bridge

*B5. Architectural Style: Deck and girder

*B6. Construction History: (Construction date, alteration, and date of alterations) 1927

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: N/A

B9. Architect: Unknown b. Builder: Southern Pacific Railroad

*B10. Significance: Theme Transportation development Area Applegate

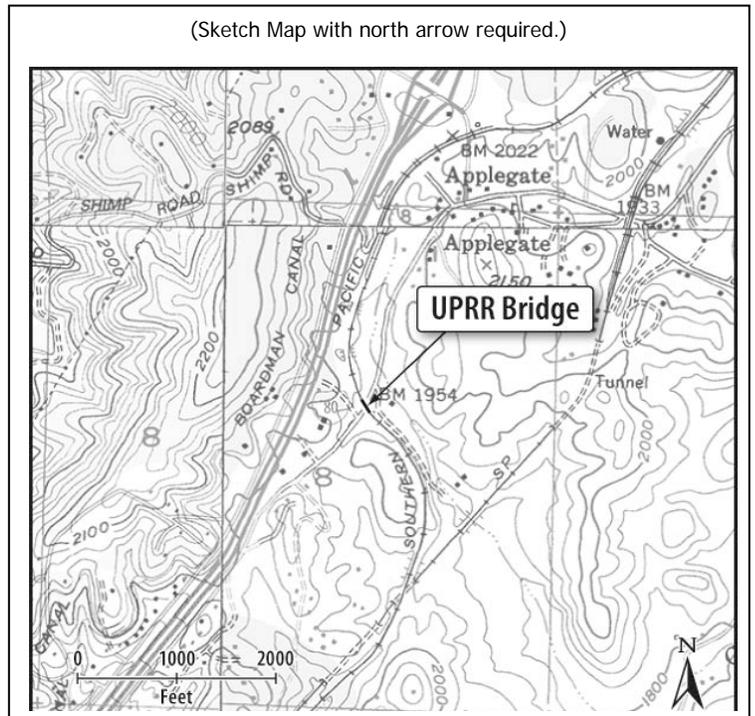
Period of Significance 1927 Property Type Bridge Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The subject SPRR bridge does not appear to meet the criteria for listing in the NRHP. The period of significance for the bridge is 1927; the year it was constructed. SPRR records indicate that the existing bridge replaced the original I-beam deck trestle structure which was built in 1909 in conjunction with State roadway improvements along what is now Applegate Road (formally part of the Lincoln Highway and Highway 40) (Caltrans 2010: Historic Bridge Inventory; Southern Pacific Company, No Date: Photographs of Bridges, 6-34-F; USGS 1950: map). The bridge is currently owned and operated by the Union Pacific Railroad (UPRR).

(See continuation sheet).

(Sketch Map with north arrow required.)



B11. Additional Resource Attributes: (List attributes and codes)

*B12. References: For full citations see Cultural Resources Inventory Report.

B13. Remarks:

*B14. Evaluator: K. Haley

*Date of Evaluation: April 2011

(This space reserved for official comments.)

***B10. Significance (cont'd):**

Although the subject bridge is located along the original western alignment of the transcontinental railroad it cannot be directly associated with the historical railroad construction endeavor. Built in 1927 the bridge is a 20th century addition to the rail alignment. As such it cannot be directly linked to the construction of the transcontinental railroad and therefore does not appear eligible for the NRHP under Criterion A. Under Criterion C, the structure is simple deck plate girder type of bridge. It was likely constructed based upon Southern Pacific standard plans for the type of bridge needed to provide an underpass for a roadway (Applegate Road). As one of many bridges of its type throughout the state, the bridge does not embody distinctive characteristics of a type, period or method of construction and does not appear to meet NRHP Criterion C. Furthermore, historic era photos of the bridge reveal contemporary changes to the structure's original deck thus affecting its integrity (Southern Pacific Company: Photographs of Bridges, 6-34-F). Overall, the subject bridge does not appear to meet the criteria for listing in the NRHP.

Additionally, the property was evaluated in accordance with Section 15064.5(a) (2)-(3) of the CEQA Guidelines, using the criteria outlined in Section 5024.1 of the California Public Resources Code, and determined not to be a historical resource for the purposes of CEQA.